

March 18, 2020

Ms. Mary K. Young, AICP
Planning and Zoning Director
Town of Westport
110 Myrtle Avenue
Westport, Connecticut 06880

Subject **Response to Traffic Peer Review – Proposed Residential Development – 41
Richmondville Avenue, Westport, Connecticut**

Dear Ms. Young:

This letter responds to traffic-related comments prepared by CDM Smith, the Town's Traffic Peer Reviewer, for the proposed repurposing of the commercial building to a residential development located on Richmondville Avenue. To assist in your review, we provide the comment (summarized) and our response.

Crash Experience

Comment – The Applicant should provide more details on the exact location of the two crashes which occurred on Richmondville Avenue relative to the site frontage. The two reported accidents occurred on Richmondville Avenue, between the Oak Street and Main Street intersections.

Response

1. One reported accident occurred at access drive at 38 Richmondville Avenue. This is based on a review of the accident diagram and narrative provided by the Town Police Department. It indicated that one vehicle was traveling north on Richmondville Avenue and another vehicle exited the driveway at 38 Richmondville Avenue. The exiting vehicle failed to grant the right-of-way at the driveway. This accident case number is identified as 2017-005225.
2. The second accident occurred on Richmondville Avenue 200 feet south of the south site access drive in front of 35 Richmondville Avenue, based on a review of the diagram and narrative provided in the accident report. It indicated that one vehicle was traveling north on Richmondville Avenue and another vehicle was exiting the driveway at 35 Richmondville Avenue attempting turn left to travel northbound on Richmondville Avenue. In this case the vehicle exiting the driveway failed to grant right-of-way to the oncoming vehicle traveling on Richmondville Avenue. This is identified as accident case #2018-003136.

Background Developments

Comment – In the Traffic Report prepared by the Applicant's Consultant there are no other developments identified in the background traffic conditions. The Applicant should confirm any discussions with the Town.

Response – Based on knowledge of other developments it was determined that the residential development located on the former Daybreak Nursery was included in the growth rate.

Intersection Sight Distance

Comment – It appears that there was no Speed Study conducted on Richmondville Avenue. The Applicant should confirm this. In the sightline plan prepared by the Applicant's Civil Engineer, the calculation for intersection sight distance (ISD) is provided based on a 25 mile per hour speed limit. However, the value shown as required is 205 feet. Based on the Connecticut Department of Transportation (CTDOT) Highway Design Manual, the required ISD for passenger vehicles exiting a minor street into a two-lane major street is 280 feet. The Applicant should verify the requirement.

The ISD was based on a 10-foot distance back from the edge of travel way. The CTDOT requirement is 15 feet back from the edge of travel way. The Applicant should depict the sightline based on this requirement.

The sightline plan shows the horizontal distance for sightline requirement but does not consider the vertical relative to the driver's eye and the object position.

Response – The Update Sited Plan prepared by Kousidis Engineering, LLC, indicates the availability of 280 feet for ISD, at a point set back 15 feet from the edge of the travel way at both driveways. However, at the existing north site driveway, to the south it is 265 feet. This is based on the posted speed limit of 25 miles per hour in the vicinity of the site along Richmondville Avenue, which would require 280 feet.

It is important to note that at the existing north access drive a portion of the building will be removed and this will have a significant benefit and improvement to visibility to the south of the northerly driveway.

To provide additional information to the Town, Speed Studies are currently being conducted along Richmondville Avenue to determine the 85th percentile speed of motorists traveling on this road in the immediate vicinity of both the north and south existing site driveways. This information will be provided to the Town as soon as possible. Based on the results of the Speed Study the ISD based on the 85th percentile speed will be identified and indicated on an updated plan.

It is important to note that both driveways are existing driveways and have been in place for many years. Accident data indicates no reported accidents at either location or in the immediate vicinity of the frontage of the site. As part of the redevelopment of the site, both driveways will be improved to include appropriate curbing, pavement markings and improved sight distance, where possible. At the existing northerly driveway a portion of the building will be removed to enhance ISD significantly to the south at this location.

At the existing southerly driveway this wide opened paved driveway area will be better defined and include a reduction in pavement width to control movements in and out of Subject Property and include curbing, pavement markings and enhancements to ISD, where possible, within the existing right-of-way of Richmondville Avenue.

It is important to note that the existing southerly driveway is an entrance only; however, as part of the redevelopment of the Subject Property this driveway will become a two-lane, two-way, access drive serving the site and main entrance to the building. The reason for providing a two-way traffic flow is to bring site traffic into and out of the site sooner to reduce possible impacts to Richmondville Avenue. This was in response to comments from residents to minimize any increase in traffic on Richmondville Avenue and near Mill Bank Road located behind the development. However, it is important to note that the comparison to the existing commercial use of the building and if fully occupied actually generates more traffic during a typical weekday peak hour condition than the proposed redevelopment to provide residential units. On a Saturday, a residential development would add more traffic since a majority of any commercial use of the building is limited.

A reduced copy of the Site Plan is attached for reference only. The full package will include a full size Site Plan package.

Richmondville Avenue

Comment – A general comment is to consider pavement markings along Richmondville Avenue along the site frontage to separate the existing building and to create an edge of roadway condition with pavement markings or curbing.

Response – A conceptual plan for Richmondville Avenue to address these issues is submitted and prepared by William Kenny Associates LLC dated March 11, 2020.

Site Access and Circulation

Comment – The existing southerly driveway is one-way (ENTRANCE ONLY). The Applicant should clarify the reason for converting the southerly driveway into a two-way driveway. As indicated earlier, there are sight line concerns for motorists exiting this driveway. In addition, the Richmondville Avenue/southerly site driveway intersection is not perfectly aligned. During the field

visit, it was noted that motorists turning left from Richmondville Avenue into the driveway are closer to the southerly edge. This creates a concern with the driveway being converted to two-way operation with exiting traffic.

The southerly portion has a two-way circulation pattern in front of the parking spaces #56-72 and a one-way pattern for parking spaces #73-76. It appears that a resident guest can use any of the numbered spaces and will circulate around to find an open space. There is a potential for conflicts with vehicles completing a against traffic entering from Richmondville Avenue. The Applicant should respond to this issue.

The Applicant should also clarify if parking spaces are designated specifically in the garage.

Is there room for vehicles to turn around in the garage (parking spaces #10-18) if no parking is available? Please clarify if these spaces are designated.

Response – As noted above, the purpose for converting the southerly site driveway to a two-way traffic flow was to provide direct access to the main entrance to the building and deliveries and to permit that delivery truck to immediately exit the site to turn to Richmondville Avenue. Further, it was converted to provide access to the lower parking area and to provide a direct exit from this parking area to Richmondville Avenue. The concern was to minimize the level of site traffic traveling behind the building and near the residence located on Mill Bank Road which is located to the rear of the site.

As part of the improvements to the site the existing southerly driveway will be improved and modified to provide an appropriate alignment of the site driveway to Richmondville Avenue. It is important to note that as part of the modifications to this existing driveway the amount of pavement area will be reduced so that it approaches Richmondville Avenue at an approximately 90 degree angle and with appropriate radii to control turning movements in and out of the Subject Property. In reference to current motorists entering the southerly driveway and driving on the wrong side of the driveway entrance, it is important to note that this driveway is currently one-way and this motorist is not on the wrong side of the road. However, in the future, this driveway will be two-way, with appropriate radii and a reduced pavement area, a STOP bar and STOP sign and a double yellow centerline to clearly designate it a two-way, two-lane driveway. We are confident that these improvements will mitigate the motorist traveling on the wrong side of the driveway if it was to be a two-way driveway configuration in the future.

In reference to the two-way traffic flow in and out of the Subject Property from Richmondville Avenue into the lower parking area, it is our opinion that there is not an alignment issue in a traffic control and traffic flow issue. This is typically found in parking areas and parking spaces located near entrance drives. However, to respond to this concern, the updated Site Plan provides a configuration, which clearly denotes the STOP bar and double yellow centerline, a painted center

Ms. Mary K. Young, AICP

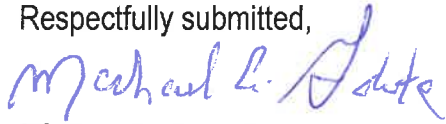
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median and better traffic control and traffic flow to clearly designate a U-turn area and to separate it as much as possible from the motorist entering the site from Richmondville Avenue.

In summary, we trust the information provided above will assist the Town in its ongoing review of this Application. The speed data will be obtained as soon as possible and the 85th percentile speed will be identified and matched to the required or desirable Intersection Sight Distance (ISD), following criteria set forth by the Connecticut Department of Transportation (CTDOT).

Respectfully submitted,



Michael A. Galante

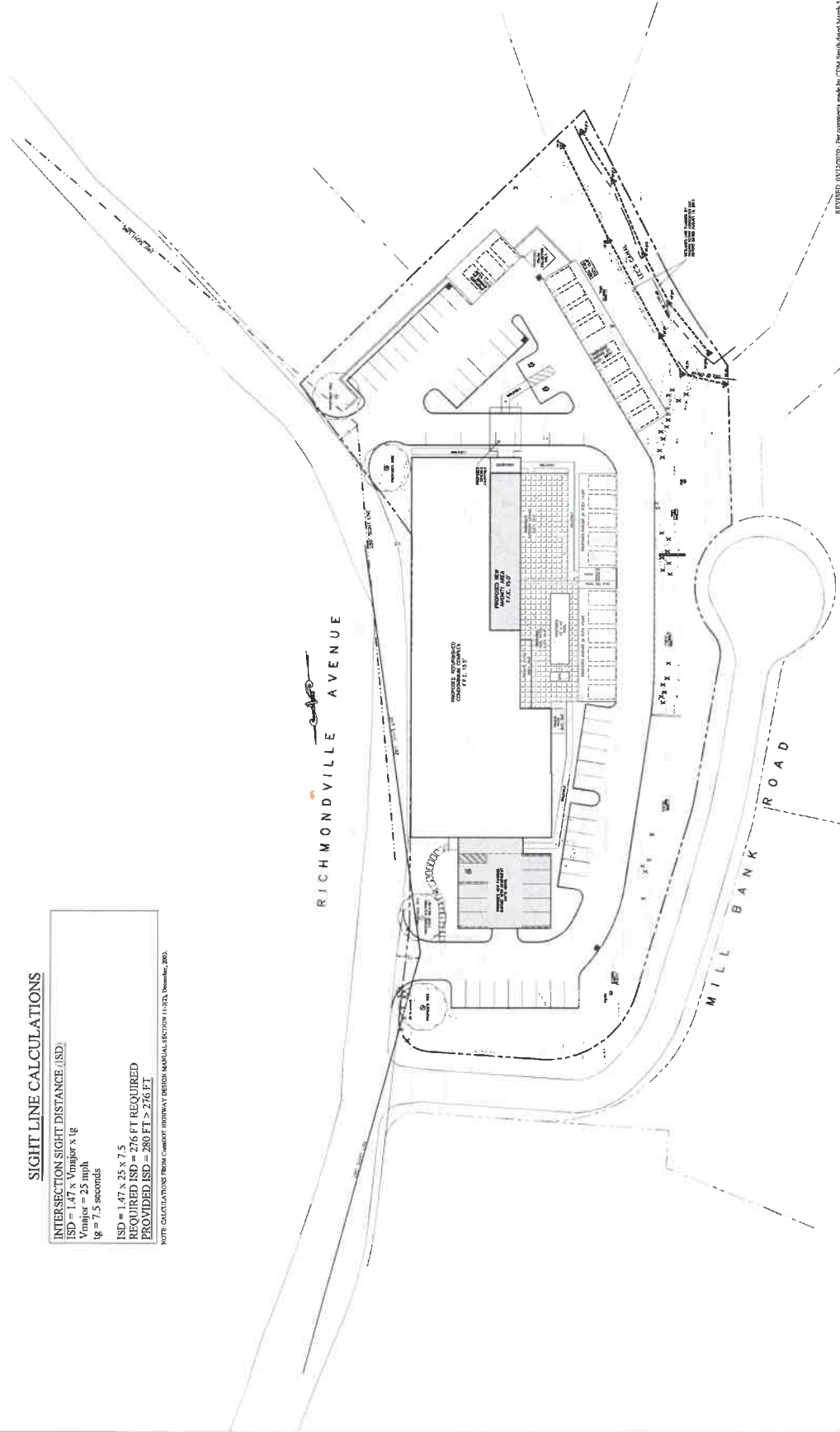
Director of Traffic

Hardesty & Hanover, LLC

Enclosure

INTERSECTION SIGHT DISTANCE (ISD):
 ISD = $1.47 \times V_{major} \times t_g$
 $V_{major} = 25 \text{ mph}$
 $t_g = 7.5 \text{ seconds}$
 ISD = $1.47 \times 25 \times 7.5$
 REQUIRED ISD = 276 FT REQUIRED
 PROVIDED ISD = 280 FT > 276 FT

NOTE: CALCULATIONS FROM CONDOT HIGHWAY DESIGN MANUAL SECTION 11-271, DECEMBER, 2003.



REVISED 03/12/2020 - Per comments made by CDM Smith dated March 5, 2020
REVISED 02/12/2020 - Per comments made by the Planning & Zoning Department
REVISED 02/04/2020 - Amended findings verbiage and associated site adjustments for Planning & Zoning Commission Submittal.
REVISED 01/11/2020 - Per comments made by the Conservation Commission and the PCB Application Review
REVISED 01/13/2020 - Amended grading & drainage systems due to the auxiliary sewer easement.

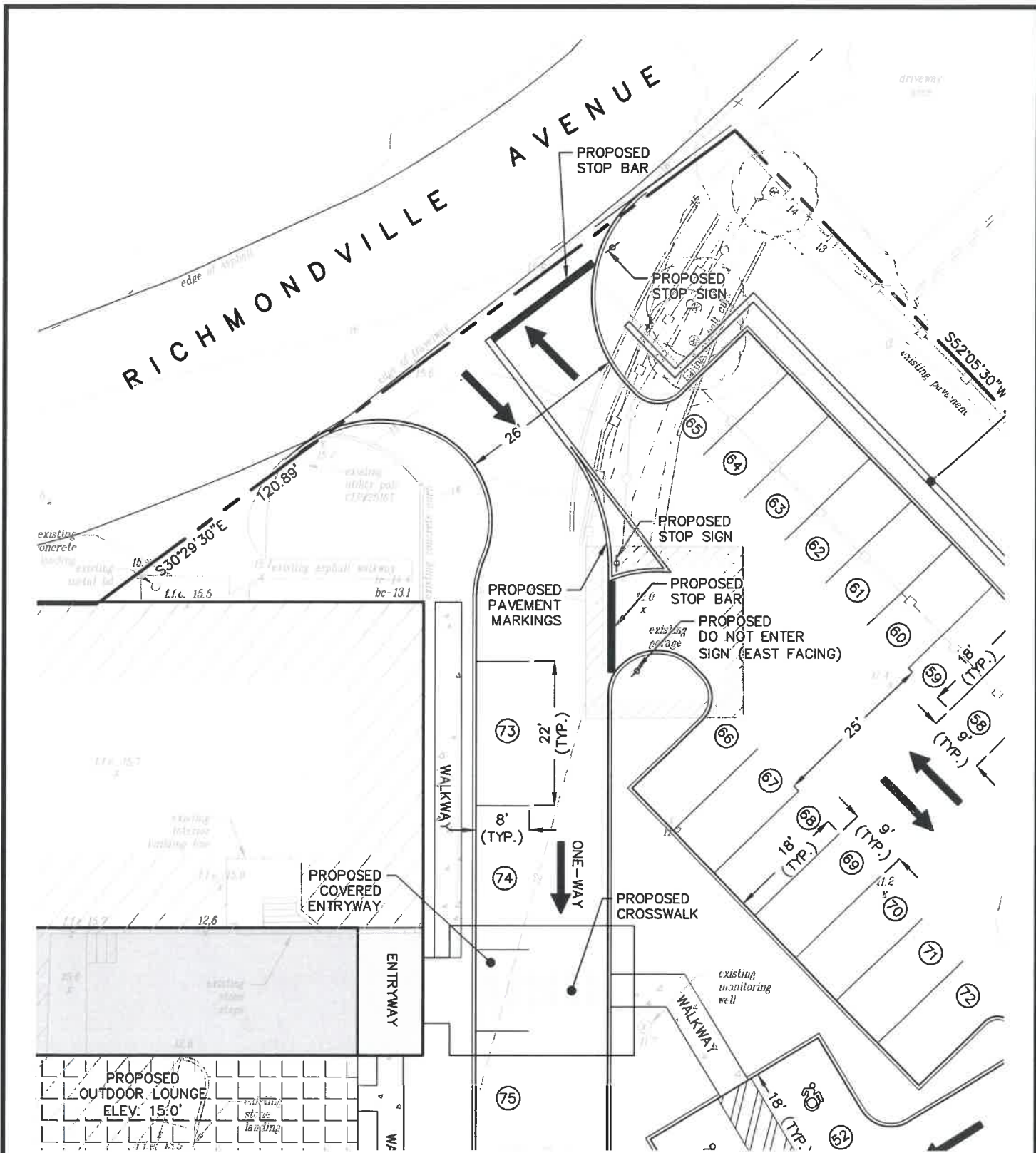
41 RICHMONDVILLE AVENUE
WESTPORT, CT

Prepared for
WELSHORTH, C. I.

SCALE: 0 30 60
DATED: 12/12/2019
JOB NO.: CH19-2

KOUSIDIS ENGINEERING, LLC
Land Development Consultants and Site Design

Land Development Consultants and Site Design
Stard, Norwalk, CT 06855
E: jim@woodengineering.com
Web: www.knowledgemanaging.com



SCALE:

1" = 20'

SOUTH ENTRANCE SCHEMATIC

41 RICHMONDVILLE AVENUE, WESTPORT, CT

PREPARED FOR

41 RICHMONDVILLE LLC



KOUSIDIS ENGINEERING, LLC

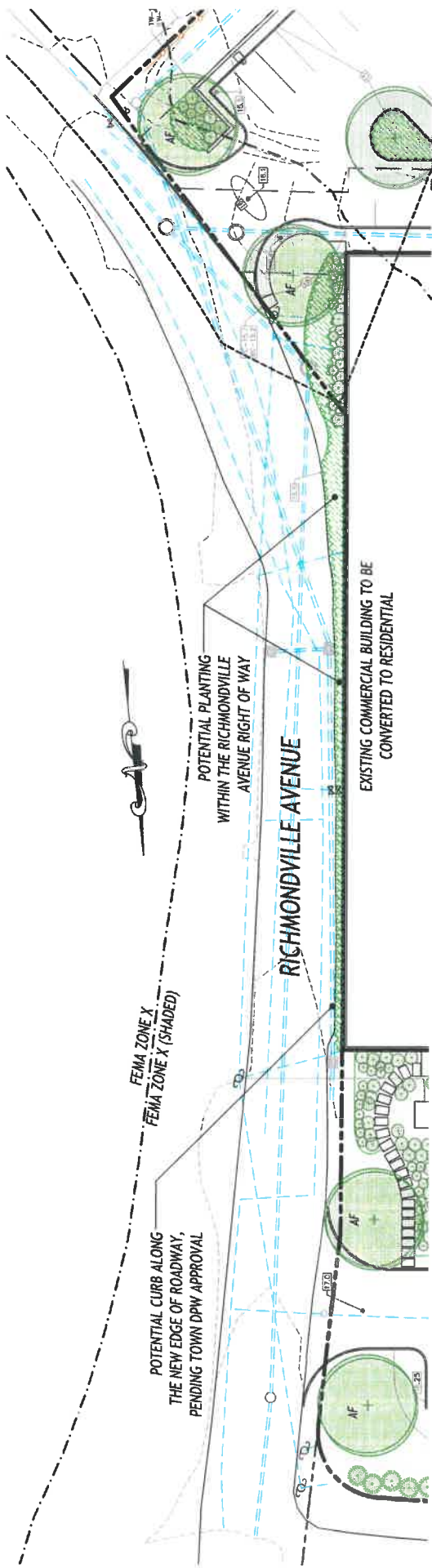
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E: jim@kousidisengineering.com

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PLANT LIST

SYM.	QTY.	SCIENTIFIC NAME	COMMON NAME	SIZE	ROOT
PROPOSED NATIVE DECIDUOUS TREES					
AF	4	ACER X FREEMANI 'AUTUMN BLAZE'	AUTUMN BLAZE MAPLE	4" CAL.	B&B
TOTAL	4				
PROPOSED SHRUBS					
HAL	15	HYDRANGEA ARBORESCENS 'INVINCIBELL LIMETTA'	INVINCIBELLE LIMETTA SMOOTHLEAF HYDRANGEA	3'-4" HT.	CONTAINER
IG	6	ILEX GLABRA 'COMPACTA'	COMPACT INKBERRY	3'-4" HT.	B&B
TOTAL	21				
PROPOSED NATIVE GROUNDCOVER					
CB	125	CAREX LAXICULMIS 'HOBB'	HOBB SEDGE GRASS	2 QUART	CONTAINER
PS	125	PHLOX SUBULATA 'WHITE DELIGHT'	WHITE DELIGHT MOUNTAIN PINKS	2 QUART	CONTAINER
PV	139	PANICUM VIRGATUM 'RUBBY RIBBONS'	RUBBY RIBBONS SWITCHGRASS	2 QUART	CONTAINER
TOTAL	389				

GENERAL NOTES

- UNLESS NOTED OTHERWISE, EXISTING AND ALL OTHER PROPOSED CONDITIONS INFORMATION TAKEN FROM A DRAWING PREPARED BY KOUSIDIS ENGINEERING, LLC.
- PROPOSED PLANTING INFORMATION PROVIDED BY WILLIAM KENNY ASSOCIATES LLC.
- POTENTIAL PLANTING WITHIN THE RICHMONDVILLE AVENUE RIGHT OF WAY TO BE COORDINATED WITH THE TOWN OF WESTPORT, CT.

LEGEND

---	EXISTING CONTOUR
---	EXISTING SPOT GRADE
---	PROPOSED CONTOUR
---	PROPOSED SPOT GRADE
---	PROPERTY BOUNDARY
---	EXISTING CATCHBASIN
---	EXISTING SUBSURFACE UTILITIES
---	PROPOSED NATIVE DECIDUOUS TREE
---	PROPOSED NATIVE SHRUBS
---	PROPOSED NATIVE GROUNDCOVER
---	LAWN OR OTHER ORNAMENTAL VEGETATION

STREETSCAPE PLANTING PLAN

PREPARED FOR:	41 RICHMONDVILLE LLC
LOCATION:	41 RICHMONDVILLE AVENUE WESTPORT, CONNECTICUT
DATE:	FEBRUARY 4, 2020 MARCH 11, 2020
SCALE:	1" = 10' 1" = 15' 1" = 30'
REF. NO.	4293

